



CURB CUT/SECONDARY DRIVEWAY APPLICATION

1685 CROSSTOWN BOULEVARD NW, ANDOVER, MINNESOTA 55304

(763) 755-8700 • FAX (763) 755-8923 • WWW.ANDOVERMN.GOV

Project Address:					
Legal Description	Lot	Block	Development		
Owner		Telephone	E-Mail Address		
Contractor		License Number	E-Mail Address		
Mail Address					
Contact Person		Telephone	Mobile Telephone		
Property Type <small>(CHECK ONE)</small>	<input type="checkbox"/> Single Family Residential	<input type="checkbox"/> Two-Family Residential	<input type="checkbox"/> Multifamily Residential	<input type="checkbox"/> Townhome	<input type="checkbox"/> Agricultural
	<input type="checkbox"/> Commercial	<input type="checkbox"/> Industrial	<input type="checkbox"/> Institutional	<input type="checkbox"/> Public / Gov't	<input type="checkbox"/> OTHER
Description or Sketch of Secondary Driveway or Curb Cut					
NOTE: <i>For requirements, see following pages. Attach a site survey / site plan or an aerial photo depicting the proposed curb cut / secondary drive location.</i>					
Signature & Acknowledgement of Contractor or Owner Authorized Agent					Date
Signature & Acknowledgement of Owner (If Owner is Builder)					Date
<i>The granting of a permit does not presume to give authority to violate or cancel the provisions of any local or state law regulating construction or the performance of construction.</i>					
BELOW - FOR CITY USE ONLY					
<input type="checkbox"/> \$600.00 Surety Deposit to Escrow Received					
NAME OF PERSON HAVING MADE SURETY DEPOSIT					
Engineering Approved for Issuance					Date

CURB CUT / SECONDARY DRIVEWAY RULES AND SPECIFICATIONS

City Street access locations must be approved by the City Engineering Department. Within existing city streets, a permit from the Building Department is required for any curb cut or secondary driveway that impact concrete curb and gutter and/or sidewalk.

County Road access locations must be reviewed and approved by the County Highway Department (763) 862-4200.

The City Permit Fee is \$50.00 and a \$600.00 surety deposit is to be held in escrow by the City until the finished work is inspected and approved by the City Engineering Department.

CURB CUT & SECONDARY DRIVEWAY LOCATIONS AND DIMENSIONS

LOCATIONS

- Unless otherwise approved by the City Engineer, all property shall be entitled to only one curb cut/access that is not directly onto a collector street or greater.
- As measured from lot lines, all curb cuts and driveways must be setback at least 60' from public street intersections.
- All commercial, industrial and multifamily use property may have one curb cut for each 125' of street frontage.
- Residential curb cuts and driveways must be setback at least 5' from side yard property lines.
- Commercial curb cuts and driveways must be setback at least 10' from side yard property lines, and must be separated from one another by at least 40'.

DIMENSIONS

- Unless otherwise approved by the City Engineer, maximum curb cut and driveway width in the boulevard (*between the edge of the road and the private property line*) must not exceed 30', or 24' on a cul-de-sac.

CURB CUT & DRIVEWAY ACCESS DESIGN & MATERIALS

DESIGN

- Curb cuts and driveways must be designed and constructed in accordance with the Curb Cut Construction Details on Page 4.

MATERIALS

- Concrete curb and gutter shall be Portland cement concrete. The compressive strength must not be less than 4,300 psi at 28 days. Mix designation shall be 3Y32 for hand placed concrete, and 3Y22 for slip form concrete. Concrete shall be air-entrained (6 percent). Asphalt patching of the roadway must use hot-mix asphalt from a plant (MnDOT Spec 2360).

SUB-GRADE

- After placement of concrete forms, sub-grade shall be tamped to assure a solid and even surface. Prior to placement of concrete, sub-grade shall be wetted down. Sub-grade shall be accurately graded to assure a uniform thickness of curbing.

FORMWORK

- Use of the edge of the existing roadway for a front form is acceptable if the road is straight and uniform. If the roadway adjacent to the new curb cut is damaged or jagged, it will be necessary to sawcut a minimum of 1' of the roadway and patch it with hot-mix asphalt along the entire length of the new curb section. Existing concrete curb must be removed to the nearest joint if less than 4' remains in place.

Forms shall be placed to line and grade established by the existing curbs. Forms shall be securely staked and braced. Form rails shall be rigid and straight. If wooden, forms shall be no smaller than 2 x 6 inches. Forms shall be cleaned and oiled before use.

JOINTS

- An expansion joint consisting of ½ inch of preformed joint filler material shall be placed at the end of the driveway section in line with the back of the existing curb. Contraction joints shall be provided at 8-foot minimum to 10-foot maximum intervals to control cracking. The contraction joints shall be formed to the full depth of the concrete using a 1/8th inch removable plate.

PLACING AND FINISHING CONCRETE

- Immediately before placing the concrete, the inside faces of the forms shall be wetted and the foundations moistened with water.

The concrete shall be placed in a manner that will prevent segregation and shall be consolidated by hand tamping or internal vibrating to fill all voids, struck off to the required grade, and floated smooth. Curb face forms and contraction joint inserts shall be removed as soon as the concrete has set sufficiently to retain its molded shape.

The top surface and face of curbs shall be hand-floated with a suitable trowel as soon after the face forms have been removed and as the condition of the concrete permits.

CONCRETE CURING AND PROTECTION

- The concrete shall be cured for a minimum period of 72 hours after the finishing operations have been completed and as soon as the set of the concrete permits. The concrete shall be covered with waterproof paper or plastic sheeting as soon as possible (without marring the concrete) after completion of the finishing operations. The curing blankets shall be in such condition and be utilized in such manner as to envelop the exposed concrete and prevent loss of water vapor. Spraying the concrete with a white membrane / curing compound is an acceptable alternative to covering with waterproof paper or plastic sheeting.

BACKFILL CONSTRUCTION

- As soon as possible without subjecting the concrete work to damaging stresses, the required backfill or embankment construction shall be completed.

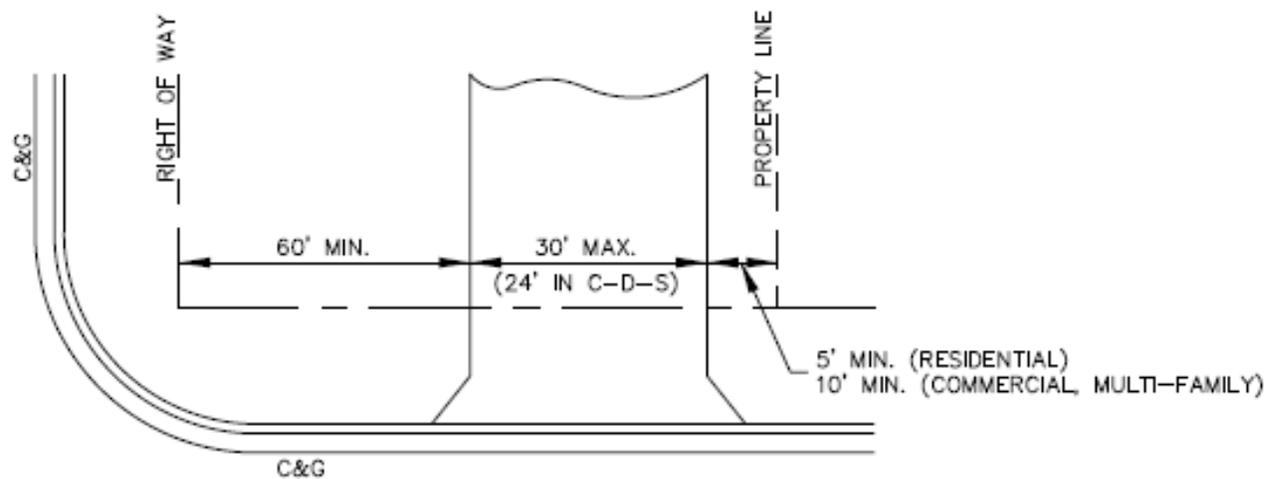
WORKMANSHIP AND FINISH

- The complete concrete work shall give the appearance of uniformity in surface contour and texture, and shall be accurately constructed to line and grade. The required joints shall show neat workmanship.

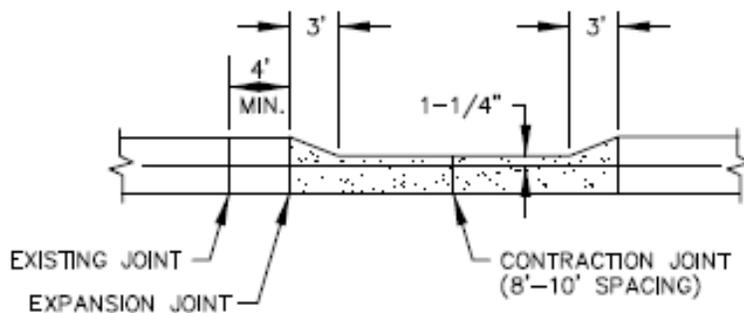
Edge and surface alignment on curved construction shall conform closely to the planned curvature, and the flow-line surface of gutters shall be finished as necessary to eliminate low spots and avoid entrapment of water.

Concrete edges and surfaces designed to straight line or grades will be checked with a 10-foot straightedge, and any deviations in excess of 5/16 inch will be considered unacceptable work.

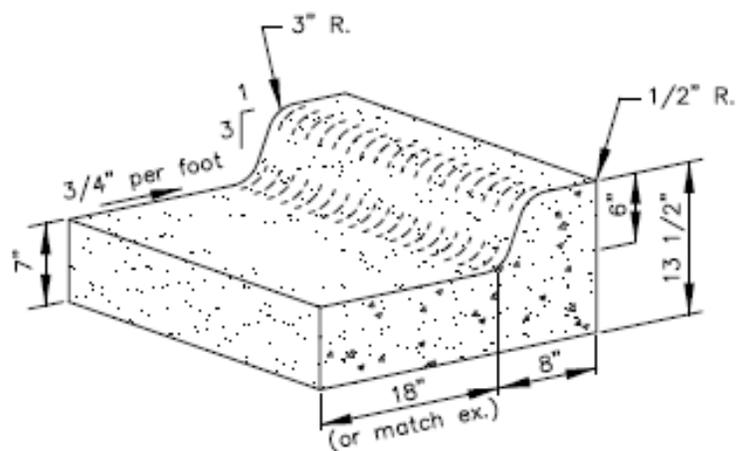
Unacceptable work shall be removed and be replaced with acceptable work as ordered by the Building Inspector. Please keep hot-mix asphalt tickets to verify that hot-mix was used for any necessary patchwork. Not providing tickets for hot-mix patching could be cause for the patch to be deemed unacceptable.



PLAN



CURB SECTION AT DRIVEWAY



B618 CURB SECTION

CURB CUT CONSTRUCTION

10/3/07



STANDARD DRAWING
NO.
518